

**INSTALLATION AND MAINTENANCE AND OPERATING MANUAL  
FOR SELF ACTUATING PRESSURE REGULATING VALVE  
(SERIES – 620)**

**CONTENTS**

<b>Section</b>	<b>Description</b>	<b>Page No.</b>
1	General	1
2	Warnings / Handling	2
3	Pre-Installation / Installation	3
4	Maintenance	4-5
5	Troubleshooting	6
6	Schematic Line Diagram of Accessories	7
7	Valve Assembly Drawings with Part List	8
8	Actuator Assembly Drawings with Part List	9

**>> 1. GENERAL**

This manual describes the instructions for handling, storage, installation, maintenance, and operation of the For self actuating PRV with spring opposed pneumatic diaphragm actuator.

Instructions not detailed in this document shall be performed in accordance with standard and safe acceptable practices as may be required by local codes, specifications and or regulations.

The instruction contained within this manual must be read before undertaking any work on the equipment supplied and if there are any questions please contact Pneucon Valves Pvt. Ltd. or its agent.

When ordering any spares please specify the unique valve serial number detailed on the equipment nameplate.

**>> 2. WARNINGS**

1. In order to avoid possible injury to personnel or damage to any equipment any 'WARNING and CAUTION' notes must be strictly adhered to. Modification of this product, substitution of non-approved components and use of maintenance procedures other than those detailed within this document could drastically affect the performance, nullify any warranty and be hazardous to personnel and equipment.
2. Personal injury, equipment damage, leakage due to escaping gas or failure of pressure containing component may result if the valve or ancillaries are over pressured or installed where service condition exceed the limits detailed in the manufacturer's specification or on the valve nameplate. To avoid such injury or damage a relief valve for over pressure protection should be provide as required by industry and sound engineering practices or applicable safety codes.
3. Due to the possibility of both elevated and low temperatures occurring on exposed surfaces of the valve assembly during operation, adequate safety precautions must be taken. This may involve insulating the valve assembly or the provision of safety clothing and equipment to avoid personal injury.
4. It is a requirement of PER 1999 that both the maximum working pressure and temperature of the valve are detailed on the nameplate. The stated values must be adhered to separately and in isolation. Attention must be paid to the combined pressure and temperature characteristics of the appropriate valve pressure containment material group as stated in ASME / ANSI B 16.34.
5. When purchased each valve / trim configuration and construction materials are specifically selected to satisfy the particular application, conditions of flow, temperature, pressure drop and controlled fluid conditions for corrosion and erosion. Included within the Pneucon Valve selection process is the consideration of the amount of power conversion allowed to reduce vibration levels within the valve and also the materials of construction are specifically selected for each application. The valve must not be used on any other application, without a review by Pneucon Valve.
6. The valve is not suitable for fire safe service and is not certified for this application or duty. Unless otherwise detailed in the quotation and order documentation the valve will not have been designed to deal with excessive seismic activity or bending moments resulting from misaligned pipe work.
7. It is not intended that externally applied lever arms will be used on the valve.

**>> 3. HANDLING**

1. The weight of the control valve assembly can cause serious injury if it is not handled correctly, using the necessary protective clothing and safety equipment. Adequate lifting equipment must be used and properly attached. Where lifting eyes are provided with the equipment these should be used to enable safe handling of the valve. All lifting equipments should be used within its defined limitations., applicable codes and safe industrial practices including (but not limited to ) the following :
  - Slings to be free of kinks.
  - Lifting hooks to be positioned directly over the load to avoid any severe swinging when lifted.
  - The operator shall ensure that the load is safe and well secured.
2. When lifting the equipment ensure that any slings or hoists are positioned to avoid any damage to the accessories or tubing mounted on the valve.
3. All eyebolts should have a shoulder, which butts against the tapped hole. Use American Drill Bushing CO ADB2300 series safety hoist rings or equivalent should be used. The following table details the safety hoist thread size and the appropriate safe loading that can be applied.

Safety Hoist Ring	Thread Size UNC	Rated Load lbf
ADB-23004	½" 13 x 0.75	2500
ADB-23002	5/8" 11x1	4000
ADB-23007	¾" 10 x 1	5000
ADB-23101	7/8" 9 x 1	8000
ADB-23105	1" 8 x 1.25	10000

>> **4. PRE - INSTALLATION**

1. The assembly should be inspected on receipt to ensure that it has not been damaged during transit. This inspection should be undertaken in a safe and controlled manner as valve equipment can be heavy and have sharp edges. Any damage found should be immediately reported.
2. If a valve assembly is required to be stored for a significant period prior to installation it must be stored in a fire resistant, clean, dry and ventilated environment to avoid damage to the valve, actuator or accessories.
3. The end caps or other protection devices should not be removed from the equipment until the time of installation, in order to help avoid any damage to the flanges and also prevent the ingress of dirt or foreign particles.
4. The adjacent pipework must be free of dirt, pipe scale, welding slag and all other foreign objects. Safe operation of the valve could be impaired by the ingress of line debris or foreign matter. It may be necessary to remove the valve or internals if the pipework is to be flushed prior to or during the commissioning period.
5. Sufficient clearance above and around the valve and actuator should be provided for access and maintenance purposes.

>> **5. INSTALLATION**

1. The valve is supplied with a flow direction arrow either cast on to the body or detailed on the nameplate. It is very important that the valve is installed into the pipework with the correct flow direction.
2. Install the valve using accepted piping practices and preferably with suitable lengths of straight pipework both upstream and downstream.
3. The preferred orientation is for the actuator to be vertically upright. If a valve is required to be installed in any other orientation the valve and actuator must be adequately supported.

## >> 6. MAINTENANCE

Maintenance such as diaphragm, gland packing or trim replacement can be done without removing the valve from the line.

### 1. REMOVAL OF ACTUATOR FROM VALVE

- Lift the valve plug off the seat ring and remove the stem coupling.
- Unscrew and remove the locking ring from the bonnet threads.
- Remove the actuator from the valve.

### 2. DIAPHRAGM REPLACEMENT

- Using the spring adjuster the spring compression should be relieved.
- Remove the diaphragm case nuts and bolts and separate the casings.
- Remove the travel indicator plate and travel plate lock nuts from the stem.

### 3. VALVE DISASSEMBLY

- Remove the gland nuts, gland flange and gland.
- Remove the body to bonnet stud nuts and lift the bonnet carefully off the valve body while holding the stem ( to prevent the valve plug assembly dropping out ).
- For cage guided valves remove the cage from the valve.
- Remove and discard the body to bonnet gasket(s).
- Withdraw the plug and stem assembly out of the bonnet.
- Remove the packing set and spacer from the bonnet.
- Removal of the seat is only necessary for replacement or machining. Where necessary the seat ring is screwed out of the body and a lathe or boring mill can be used for seat rings that are difficult to remove. For extremely tight seat rings the valve body should be heated to assist removal.
- For top guided valves the upper guide is pressed into the bonnet and does not normally require removal. When removal of the guide is essential it should be undertaken in a machine.
- All the parts should be cleaned and examined for damage.

#### 4. VALVE ASSEMBLY

- New plugs are normally supplied complete with stems already fitted. If the replacement parts are separate the stem should be screwed into the plug. Drill the plug and stem, countersink the drilled hole, fit the pin and peen. The pin should then be machined flush to the plug.
- Apply sealing compound to the seat ring and install the seat ring into the body. The seat ring should be tightened.
- Place a new gasket on the body and install the cage for cage guided valves.
- Install the plug and stem assembly.
- For cage guided valves place a new gasket on the cage.
- Lower the bonnet carefully over the plug stem and also over the body studs. The bonnet should be centered to ensure that the plug and stem assembly moves freely.
- Fit the body stud nuts and tighten evenly. The plug and stem assembly should be moved in a linear direction only to ensure that it moves freely.

#### 5. PACKING BOX ASSEMBLY

- Ensure that the packing box and gland assembly components are all clean.
- A new stem packing should be fitted.
- Fit the spacer into the packing box.
- Fit the stem seal by individual rings ensuring that each ring is fully installed.
- Fit the gland follower ensuring that it enters the packing box by at least 3mm
- Place the gland flange onto the gland follower and evenly tighten the gland flange nuts.

#### 6. SEAT AND PLUG LAPPING

- Lapping of the plug to seat (if required) should be carried out with the valve fully assembled and without the actuator being fitted.
- Produce a lapping tool. One option is to weld a nut onto a bar, which matches the stem thread.
- Apply a lapping compound to the seat seating surface only and apply a light oil in the packing box area.
- Lap with short oscillating strokes.
- Raise the plug occasionally, lower it to another position and continue the oscillating strokes to ensure an even lapping over the entire seating surfaces of the plug and seat.
- Disassemble the valve, clean the parts and reassemble the valve using the procedure detailed in section 6.4.

#### 7. FITTING ACTUATOR TO VALVE

- Lower the actuator over the plug stem and gland flange to seat squarely on the valve bonnet.
- Rotate the actuator to the required position, screw the locking ring onto the bonnet and securely tighten.
- To connect the actuator to the valve stem the valve plug must be on its seat and the actuator in the lowest travel position. Apply one half of the stem coupling to both the valve and actuator stem. Apply the second half of the coupling carefully engaging the threads, then insert the coupling cap screws and hand tighten. The actuator should then be lift off its seat and the plug stem rotate one full turn in the coupling to provide a seating load. Securely tighten the coupling cap screws.

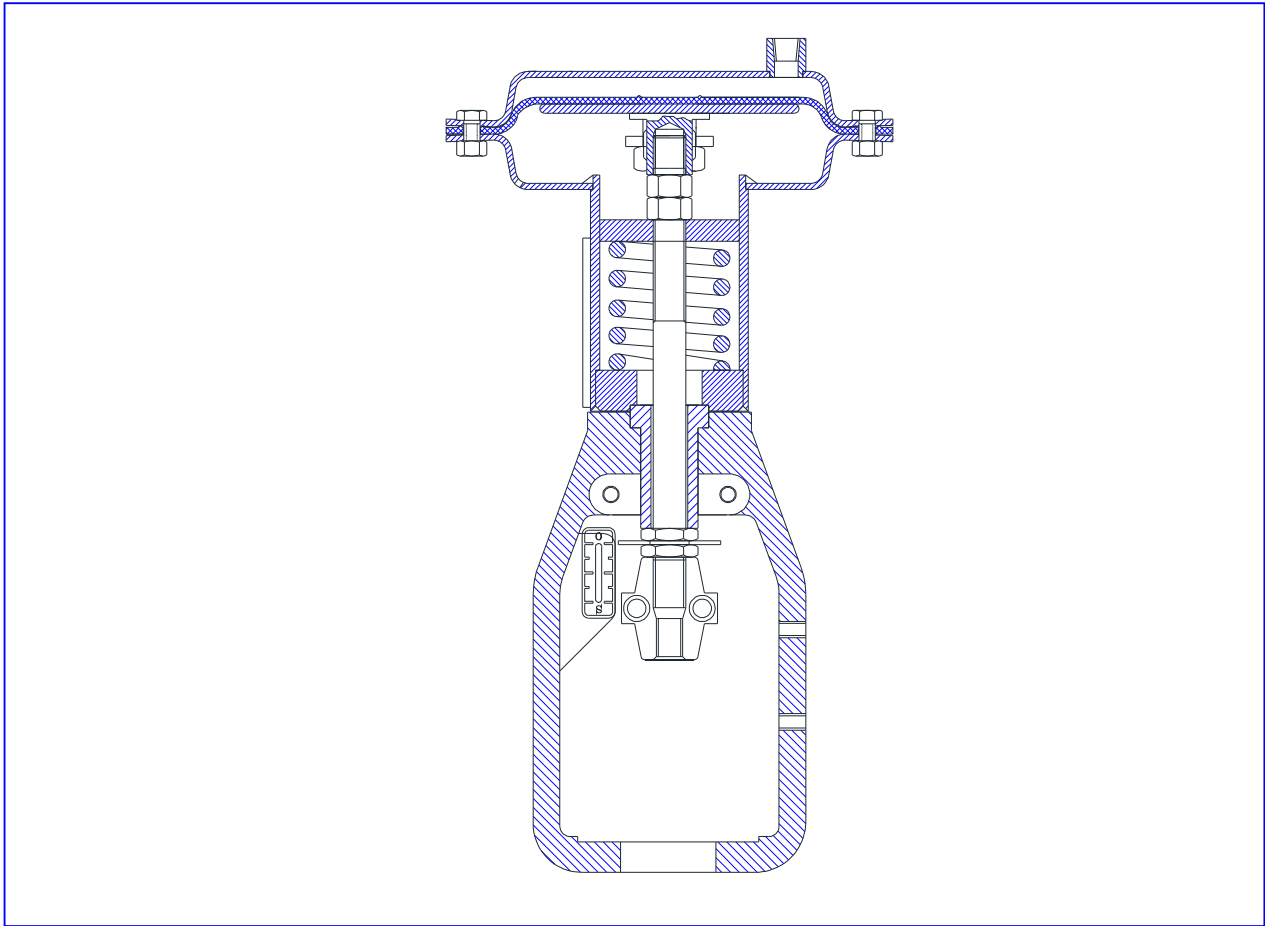
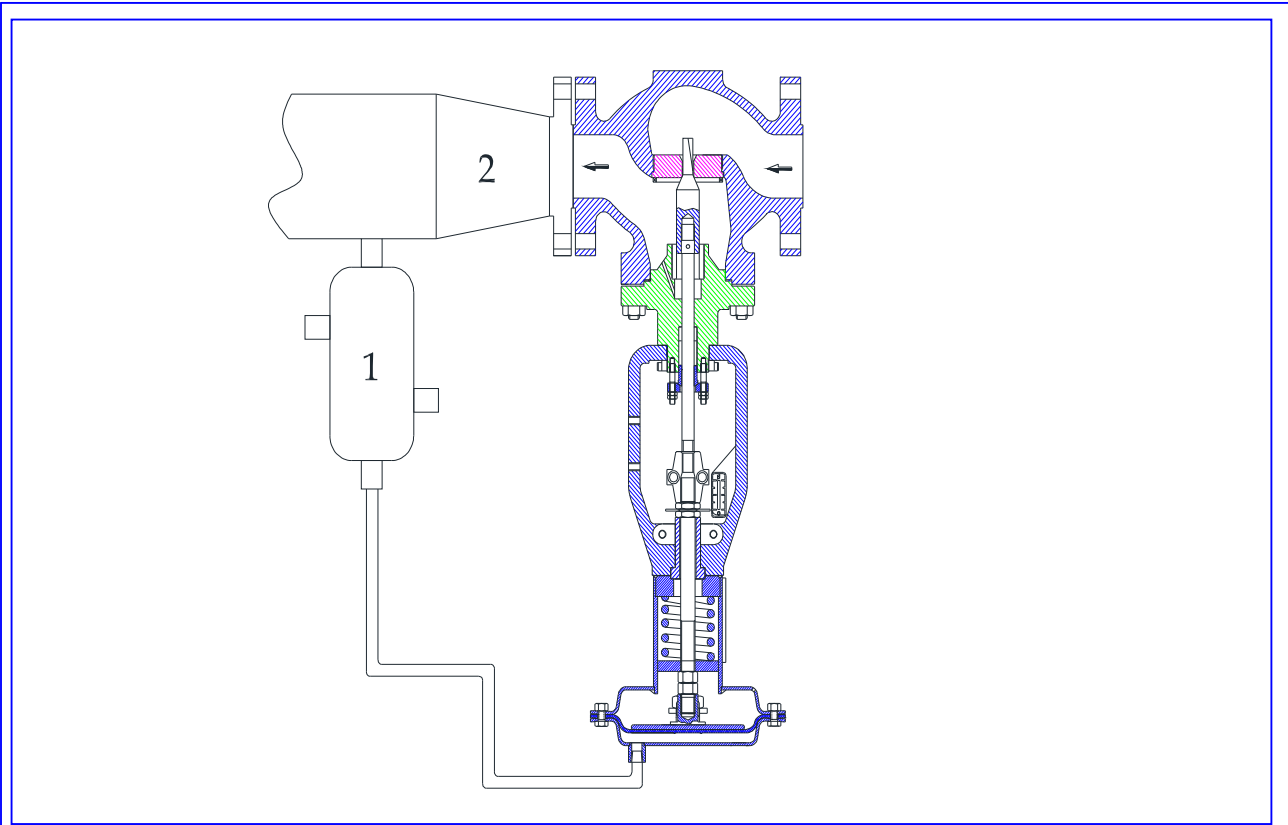
### 8. FITTING ACCESSORIES

- Condensor pot shall be connected in between the feedback point on the downstream line and the sensing diaphragm.

### >> 7. TROUBLESHOOTING

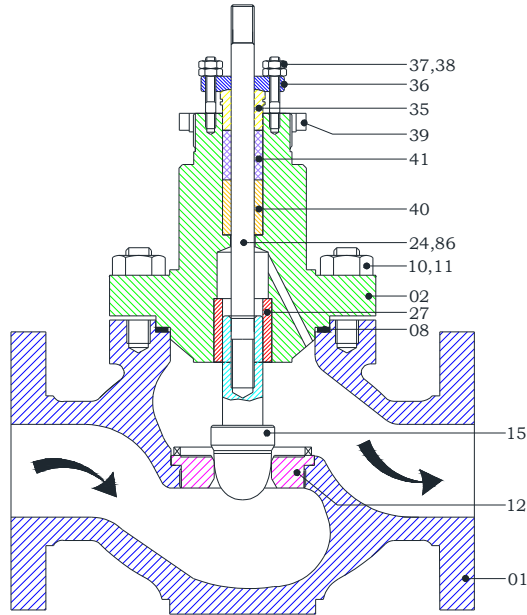
PRV normally require seldom attendance, except for routine check .

SYMPTOMS	PROBABLE CAUSES	CORRECTIVE ACTION
Excessive leakage when valve closed	Worn or damaged seat ring or plug	Disassemble and replace damaged part as detailed in maintenance manual
	Inadequate actuator thrust	Check actuator spring adjustment and air supply Reconfirm service conditions and contact factory
	Actuator leakage	Check for medium leaks with soapy water. Tighten or replace connection / seals.
Valve operation not smooth	Gland flange over tightened	Loosen gland flange nuts and retighten to finger tight
	Alignment of valve and actuator stem	Check actuator is square and tight on bonnet mounting
	Actuator faulty	Service actuator as detailed in maintenance manual
Excessive noise	Flashing or cavitations	Check that trim is suitable for the application. Refer to factory
	Loose supports	Tighten the supports sufficiently
	Damaged trim assembly	Replace the trim parts



**BUILT IN RELIABILITY**

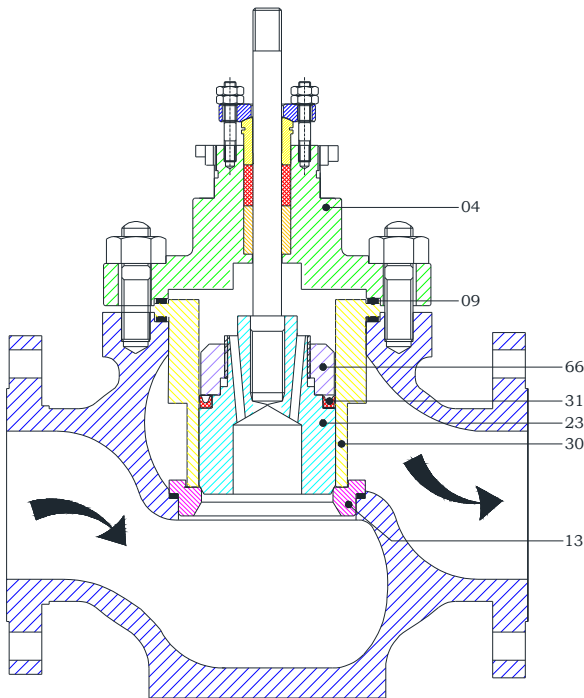
### GLOBE 2 WAY CONTROL VALVE WITH CONTOURE TRIM



NAME OF PART	PART No.
BODY	01
BONNET STANDARD	02
<b>BODY JOINT GASKET *</b>	<b>08</b>
BODY STUD	10
NUT FOR BODY STUD	11
SEAT RING	12
PLUG CONTOURE	15
PLUG STEM	24
PLUG STEM PIN	86
PLUG GUIDE	27
GLAND	35
GLAND PUSHER	36
GLAND STUD	37
NUT FOR GLAND STUD	38
LOCKING RING	39
PACKING SPACER	40
<b>GLAND PACKING *</b>	<b>41</b>

\* - RECOMMENDED SPARES

### GLOBE 2 WAY CONTROL VALVE WITH CAGE GUIDED TRIM

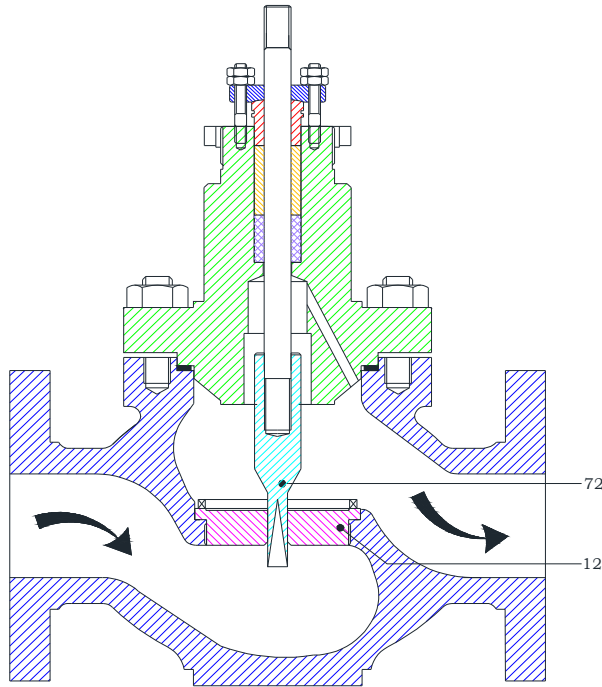


NAME OF PART	PART No.
BONNET STANDARD - LN	04
<b>BONNET JOINT GASKAT *</b>	<b>09</b>
SEAT RING - LN	13
PLUG BALANCE	23
<b>LIP SEAL / CARBON RING *</b>	<b>31</b>
RETAINER NUT	66
CAGE BALANCE	30
OTHER PARTS ARE AS PER CONTOUR TRIM	
<b>Safety Precautions :</b> Use strainer to remove line debris to protect any damages to valve internals, so as to have smooth movement of plug in cage.	

\* - RECOMMENDED SPARES



**GLOBE 2 WAY CONTROL VALVE WITH MICROSPLINE TRIM**



NAME OF PART	PART No.
SEAT RING	12
PLUG MICRO SPLINE	72
OTHER PARTS ARE AS PER CONTOUR TRIM	

In order to avoid possible injury to personnel or damage to valve parts, WARNING and CAUTION notes must be strictly adhered to. Modifying this product, substitution non- factory or inferior parts, or using maintenance procedure other than outlined could drastically affect performance, void product warranties and be hazardous to personnel and equipment.

While ordering spares, please indicate 'Valve Serial No.' appearing on 'Name Plate' fixed on the Actuator.

The Company's policy is one of continuous product improvement and the right is reserved to modify the specifications contained herein without notice.



**PNEUCON VALVES PVT LTD**

Plot No: A-35, Road No. 10, Wagle Industrial Estate,  
 Thane – 400 604, India.  
 Phone: +9122 49620350 / 60, Fax : +9122 2583 8373  
 E-Mail: [sales@pneuconvalves.com](mailto:sales@pneuconvalves.com)  
 Web: [www.pneuconvalves.com](http://www.pneuconvalves.com)

PVIM-620 Designed By : Rajesh

***BUILT IN RELIABILITY***

